

**Transit Oriented Development:** "...this zone should allow for sufficient density to support a transit oriented community focused toward residents, workers and visitors that seek the convenience of transportation facilities in a walkable community framework."

**Beacon Comprehensive Plan, 2007**



Dia: Beacon

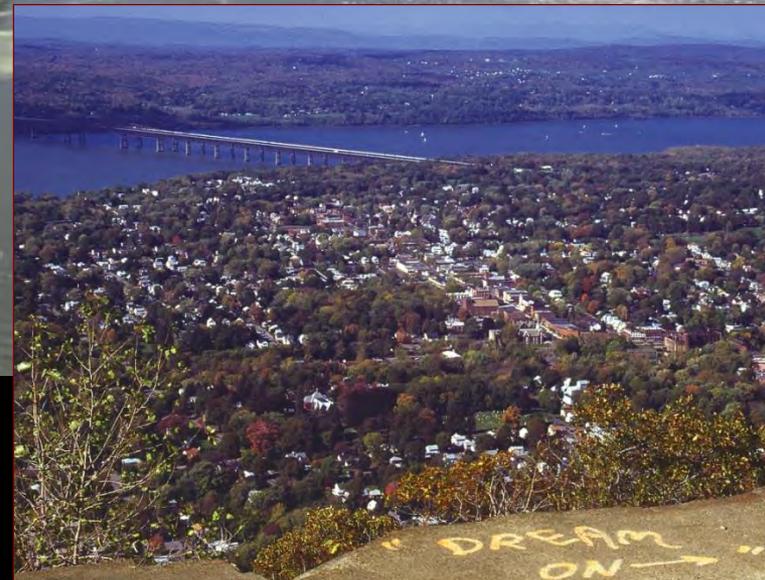
Dennings Point State Park  
60+ Acres

Train Station

Long Dock Park  
16 Acres

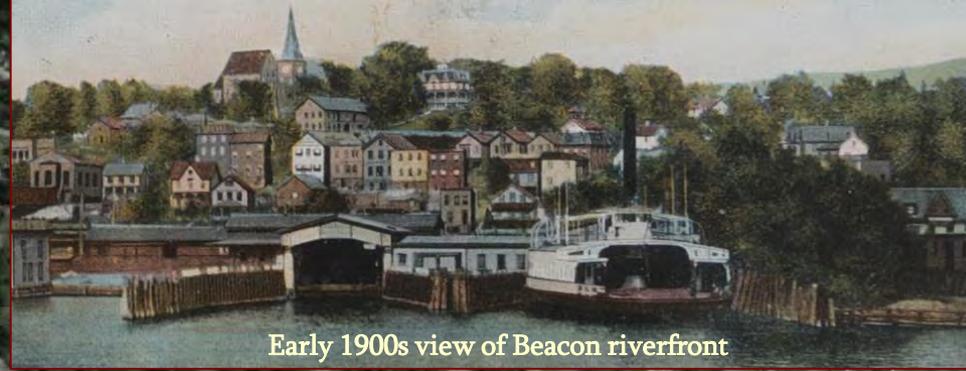
Ferry Dock

Riverfront Park  
11.5 Acres



**City of Beacon**

West End of  
Main Street



Early 1900s view of Beacon riverfront

1/2-Mile

Wolcott Ave

Train  
Station

Beacon

Dia:Beacon

Riverfront  
Park

Long Dock Park

299 Units Under Construction  
198 Units Approved by Planning Board  
412 Units Now Before Planning Board  
327 Units Under Consideration

# Centers and Greenspaces Plan City of Beacon

April 2007



## Legend

- Neighborhood Center  
1/4 - 1/2 Mile Radii
  - Emerging Center
  - Easement-Protected and Public Lands
  - Major Greenspace Parcels
  - Parcel Lines
  - Municipalities
  - Water/Wetlands / Floodplains
  - Trails
  - Main Public Places
  - Prime Redevelopment Opportunities
- 1/4 Mile Radius: Park Once and Walk Around\*  
Mixed - Use Core
- 1/2 Mile Radius: Walkable from Edge to Center,  
Primarily Mix of Housing Types

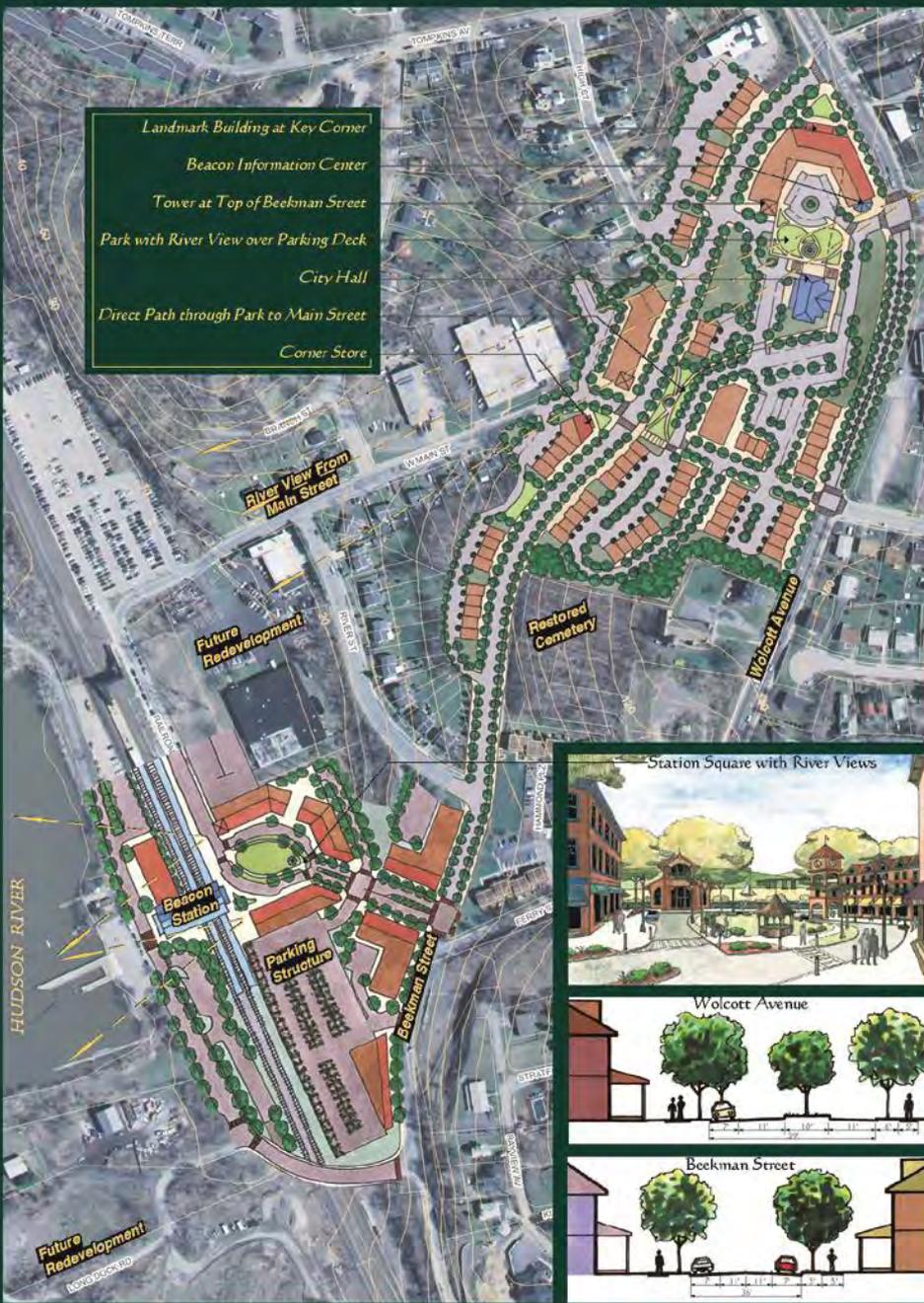
Dutchess County Department of Planning & Development  
27 High Street  
Poughkeepsie, NY 12601  
845.486.3600



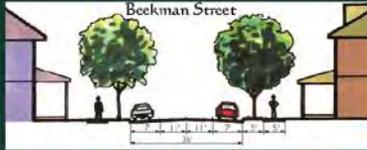
Scale: 1" = 1500 Feet

Putnam  
County

# 2007 Comprehensive Plan



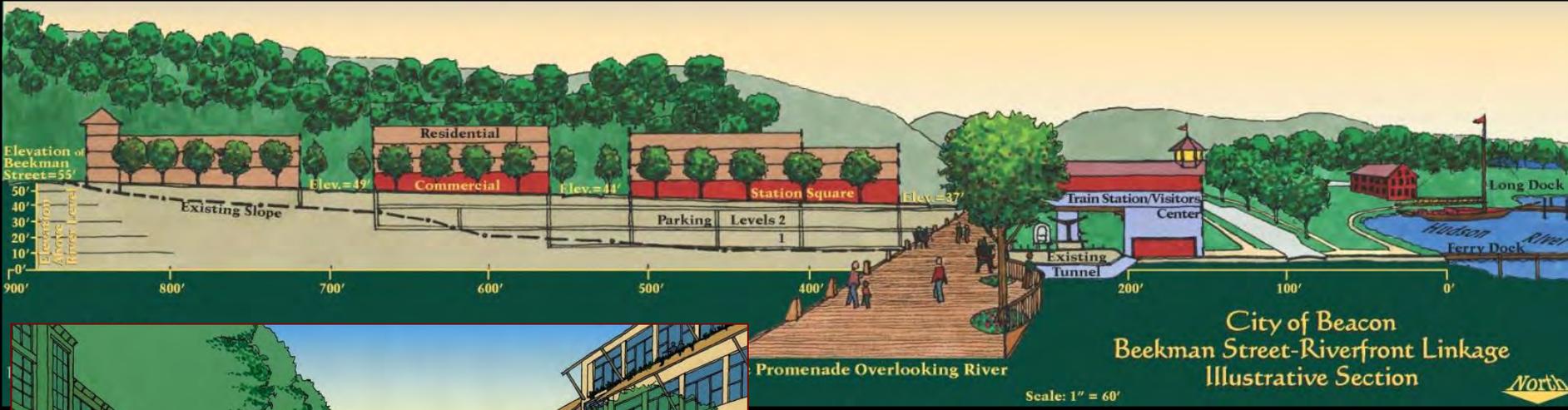
- Landmark Building at Key Corner
- Beacon Information Center
- Tower at Top of Beekman Street
- Park with River View over Parking Deck
- City Hall
- Direct Path through Park to Main Street
- Corner Store



Connecting Beacon's Main Street with the Hudson River and Railroad Station



Existing View Looking South



Elevated Station Square and Promenade with Expansive River Views

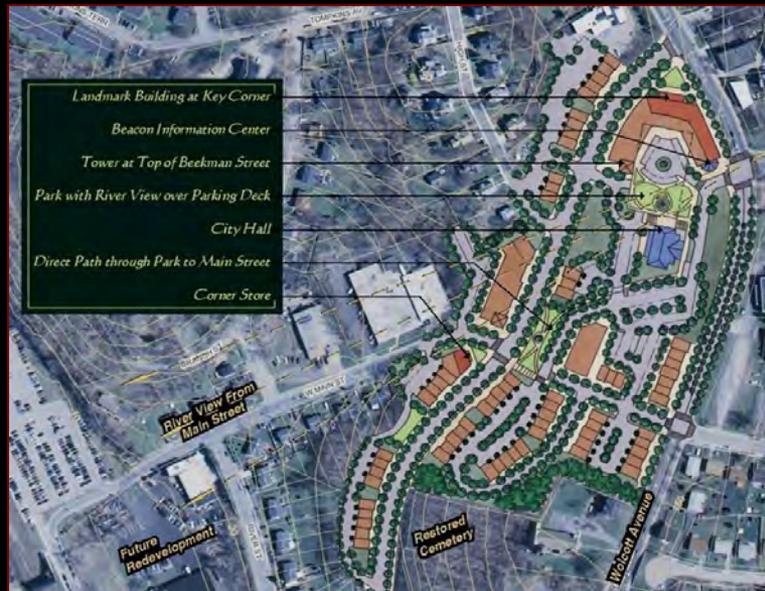
# City of Beacon Riverfront Illustrative Sketches 2010

## Conventional Zoning:

- Text, Tables, and Map
- Negative Restrictions
- Focus on Numbers, Single Parcels, and Parking Lots
- Separated Use Districts

## Form-Based Codes:

- Design Illustrations
- Positive Examples
- Emphasis on Urban Form and Streetscape Context
- Mixed-Use Neighborhoods



Illustrative Linkage Plan

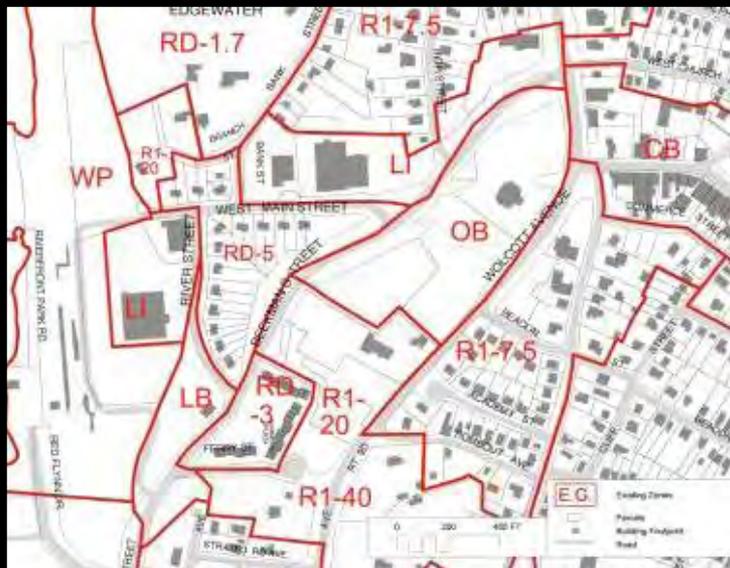


Building Form Standards

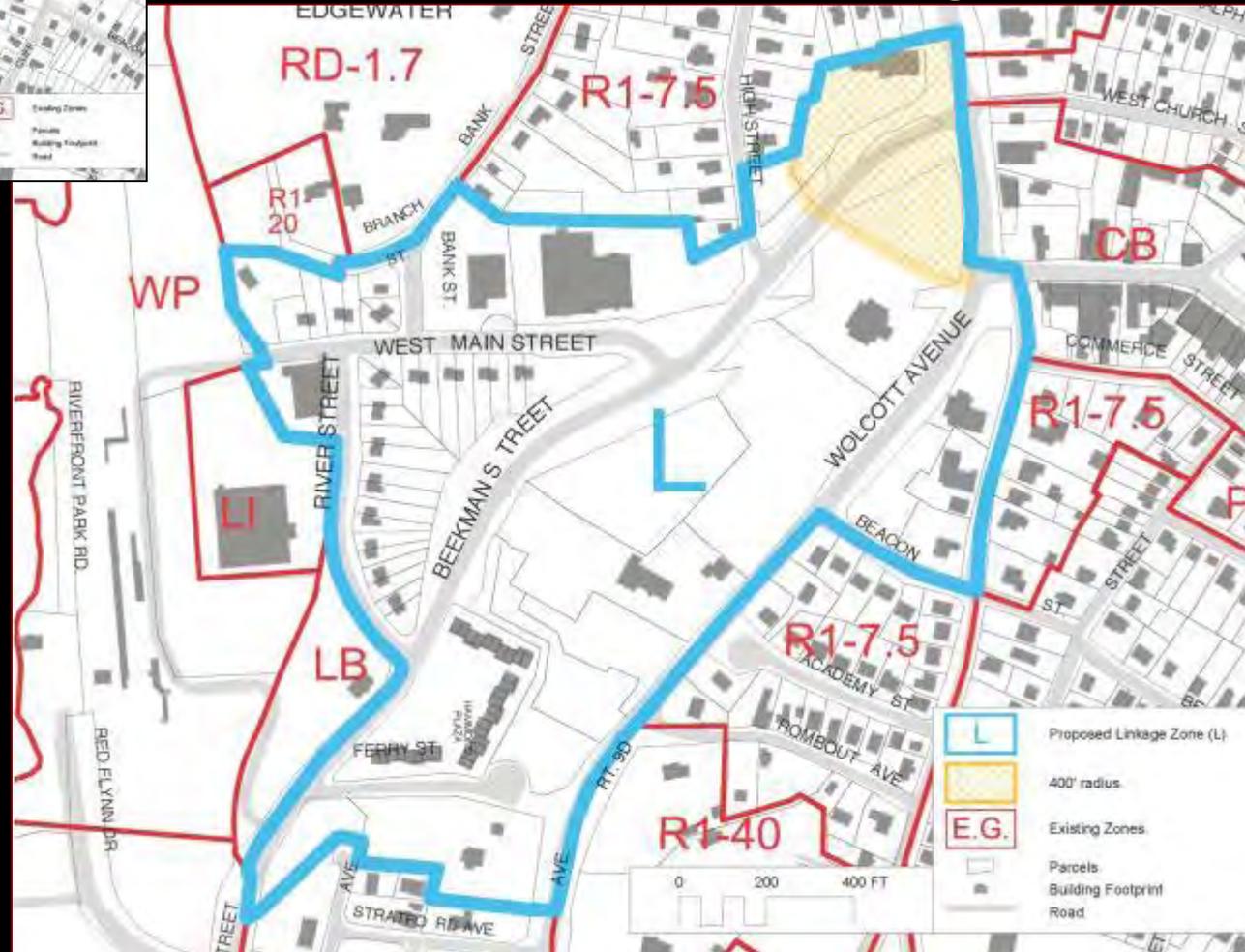


Public Space Standards

## Previous Zoning Districts



## Adopted Linkage District (L)



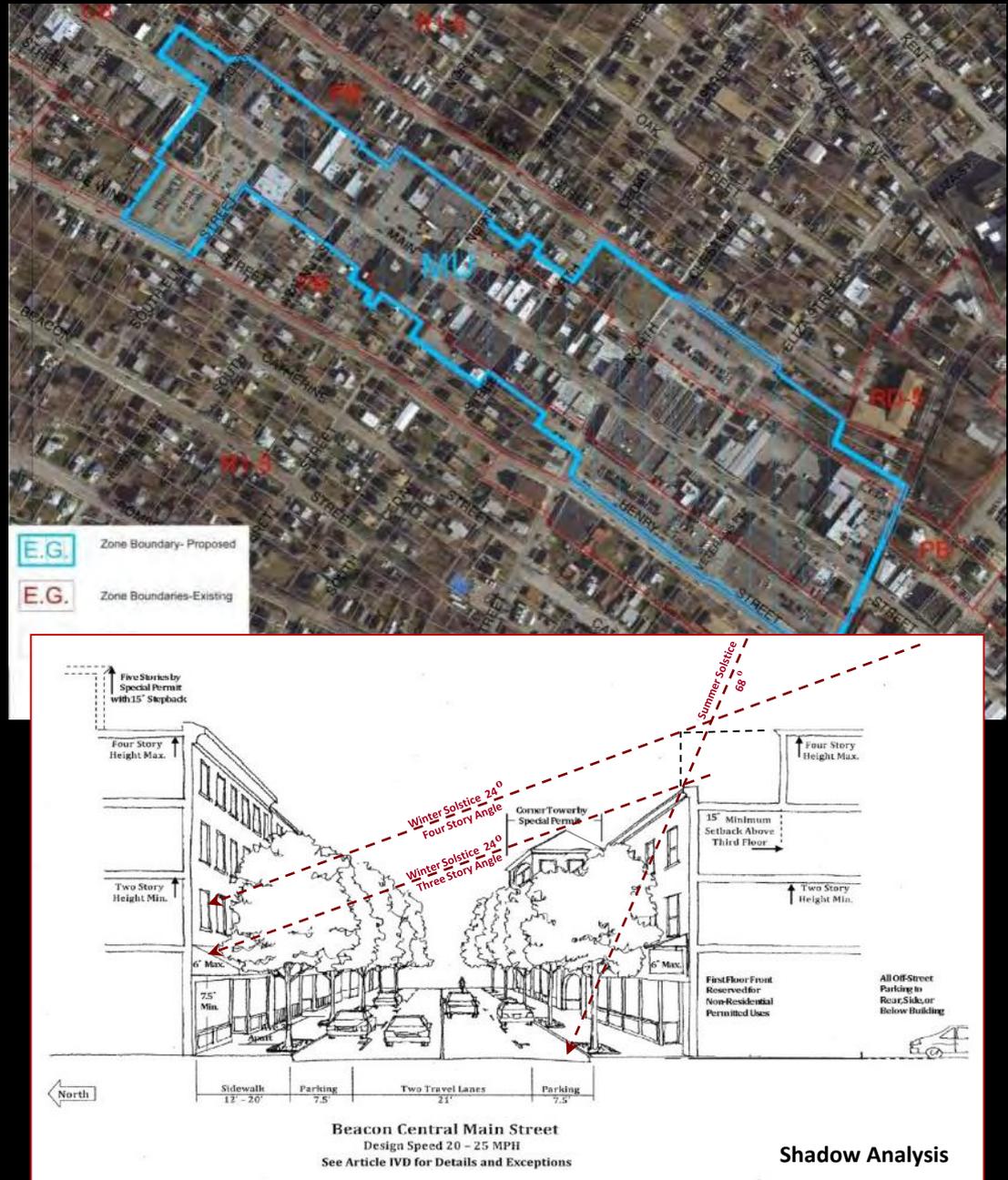
### General Approach:

- Replace 7 previous zones with one integrated district
- Permit taller buildings
- Allow retail/restaurant uses only at the main intersection
- Reduce parking requirements

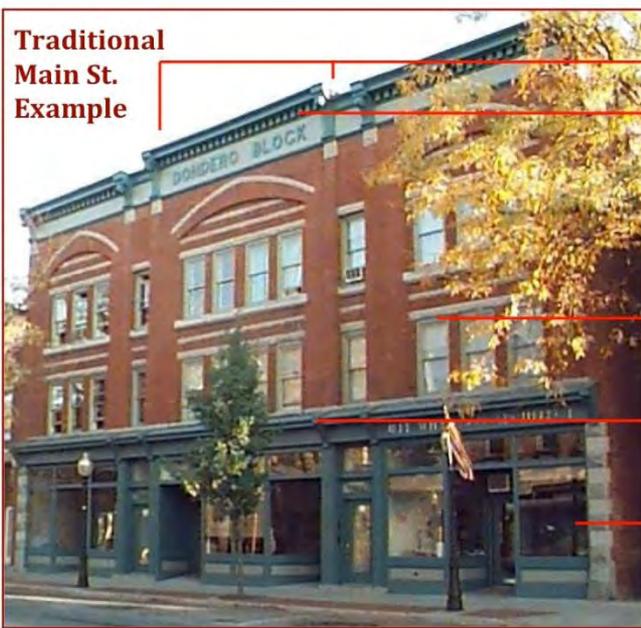
# Central Main Street District (CMS)

## General Approach:

- Use East End and West End historic districts as models
- Encourage mixed-use infill
- Permit taller buildings with consideration of street shadows
- Prohibit auto-oriented uses, gas stations, and drive-thru lanes
- Reduce parking requirements and screen off-street lots to rear



**Traditional  
Main St.  
Example**



Façade and roof line breaks at intervals of no more than 35'

Top floor cornice feature

Bay windows, balconies and open porches may encroach up to 4' over the sidewalk

Primary window proportions greater in height than in width

Secondary storefront cornice or first floor articulation

Commercial first floor facing Main Street

**More Modern  
Example**



## **Design Standards Consistent Examples**

Wood, brick, stucco, stone, or fiber-cement siding and trim recommended

Metal, glass, or canvas-type awnings and canopies or projecting signs may encroach up to 6 feet over the sidewalk above 7 feet

Street trees planted on average 30' - 40' apart

Commercial buildings shall have at least 70% glass on first floor facades between 2' and 10' above the sidewalk



## **Design Standards Inconsistent Examples**



Two-story minimum required, allowing second floor occupancy

Architectural features and windows should be continued on all sides, avoiding any blank walls

Vinyl, aluminum, or sheet metal siding or sheet trim shall not be permitted



Buildings should have a top floor cornice feature

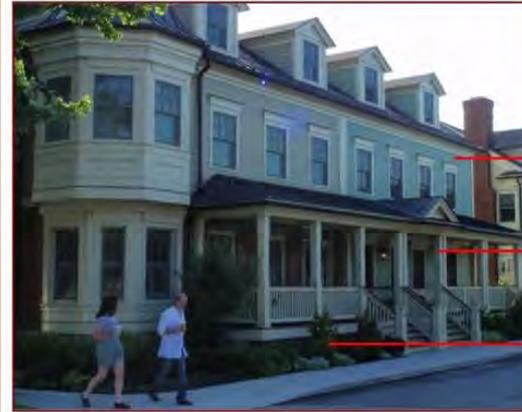
Primary window proportions shall be greater in height than in width

Commercial buildings shall have at least 70% glass on the first floor facade



Vinyl and aluminum awnings shall not be permitted

## **Design Standards Consistent Examples**



Wood, brick, stucco, stone, or fiber-cement siding and trim recommended

Bay windows, balconies and open porches may encroach up to 4' into the setback

Required landscaping between the sidewalk and building to enhance the streetscape



In the Linkage District, a setback of at least 15' behind the façade above the third story

Primary window proportions greater in height than in width

Secondary cornice or first floor articulation

Residential buildings shall have at least 30% glass on first floor facades



Top floor cornice feature

Façade and roof line breaks at intervals of no more than 35'

Street trees planted on average 30' - 40' apart

Metal, glass, or canvas-type awnings and canopies may encroach up to 6 feet over the sidewalk above 7 feet

Existing Conditions



Main Street



Dutchess  
County  
Center

114 Spaces

7,750/sf Commercial  
16 Housing Units

3 St.

16 Spaces

Café Tables

2 St.

3 St.

4 St.

3 St.

4 St.

8,750/sf Commercial  
25 Housing Units

47 Spaces

S. Elm Street

Dewindt Street

## Beacon County Parcel Illustrative Sketch Plan

- 8,500 Square Feet 1st Floor Commercial
- 26 Housing Units on Three Upper Floors
- Pocket Park and 114-Space Parking Lot

North  
1" = 60'

# Adopted Central Main Street District (CMS)



Approved 4-Story  
Building - 28 Units  
1st Floor Commercial

4-Story Building 18 Units  
1st Floor Commercial  
Under Construction

Existing Building

Proposed Building

# Adopted Linkage District (L)





WD District

Linkage District

WD District

2017 Comprehensive Plan Update

# Waterfront Development North (Metro North Parking Area)



"Green Screen" base of building to cover and enhance parking levels



Profile Section Cuts & Building Height Areas

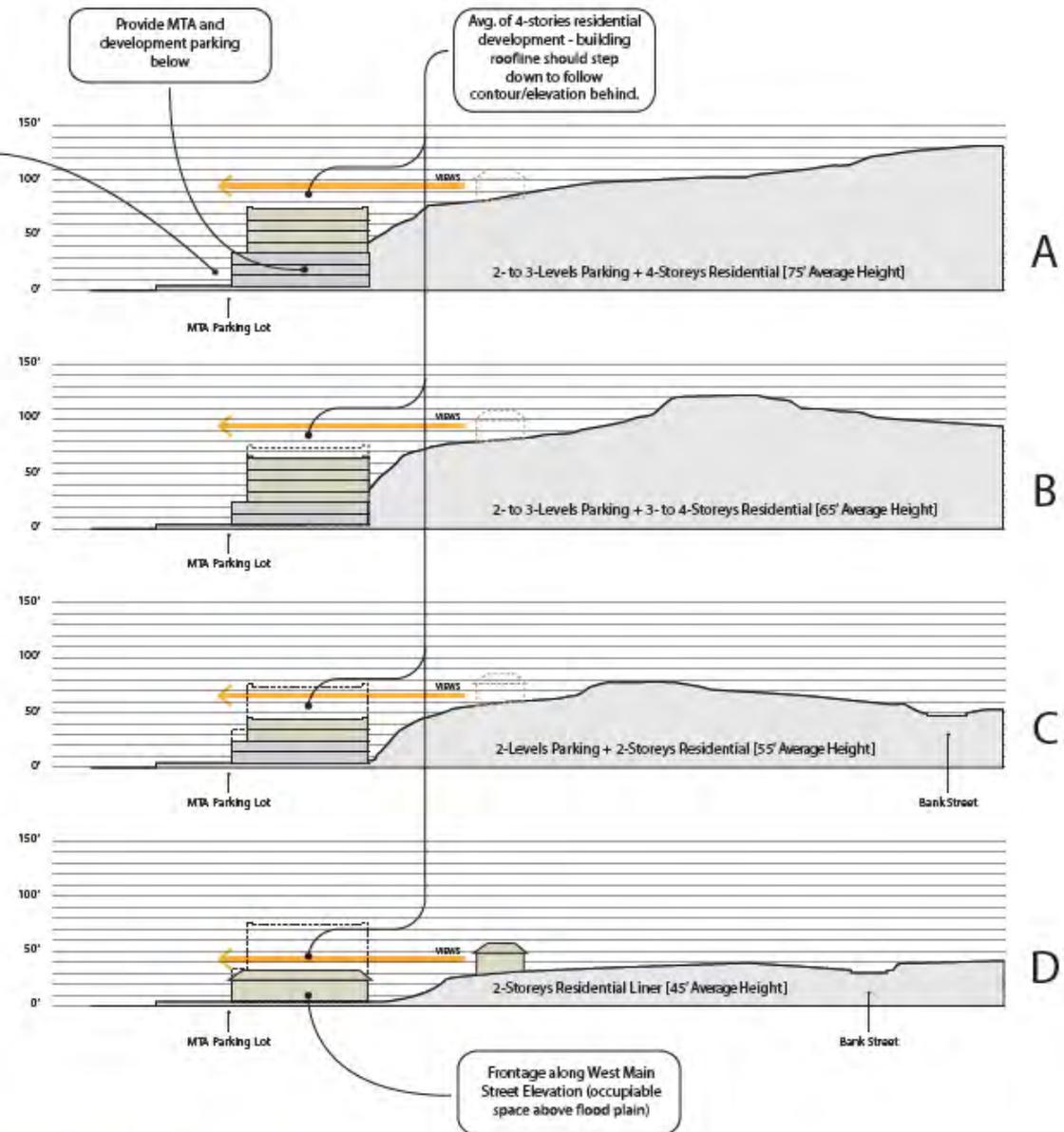


FIGURE 10-4: WATERFRONT/STATION AREA DEVELOPMENT PROFILE (NORTH SITE)

**Waterfront Development South  
(Metro North Parking Area)**



Profile Section Cuts

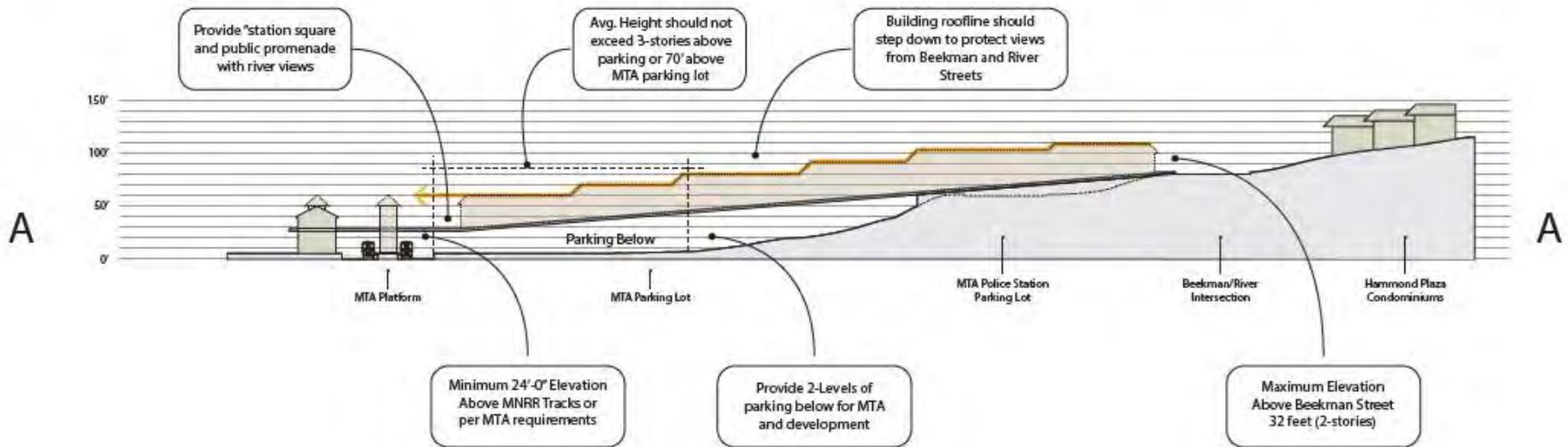


FIGURE 10-5: WATERFRONT/STATION AREA DEVELOPMENT PROFILE (SOUTH SITE)

# Complete Streets Guidelines Adopted December 2016



AARP Bulletin

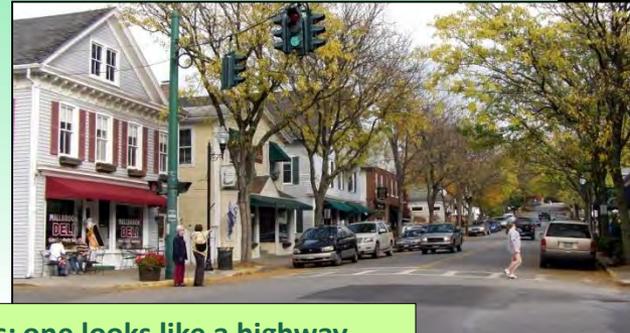
A Complete Street is a comfortable social space that is safe and convenient for walking, biking, cars, and transit regardless of age or ability.



Main Street Sharrows

**SLOWER, SAFER STREETS**

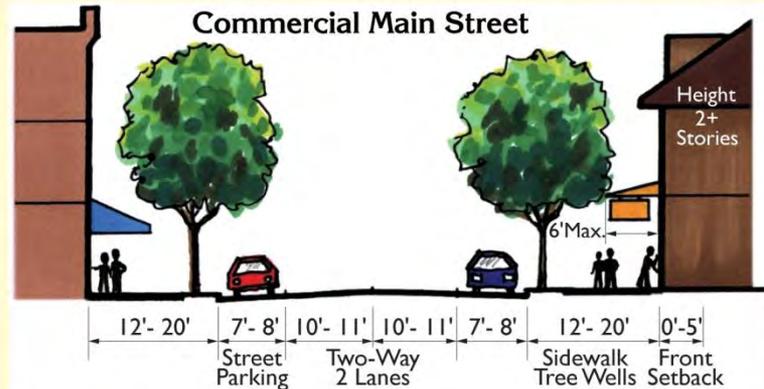
*Design narrow streets in cities, villages, and hamlet centers with buildings close to sidewalks, street trees, and other pedestrian-friendly features that promote slower speeds.*



Compare two main streets: one looks like a highway, while the other balances car access with walkability.



Main Street, Beacon



- DESIGN SPEED 20 - 25 MPH
- STREET LIGHTS 15' MAX. 40'- 60' O.C.
- CURB RADIUS 15'- 25'
- EXTENDED CURBS AT CROSSWALKS
- STREET TREES 25'- 30' O.C.
- RIGHT-OF-WAY 56' MINIMUM

### BUILDING BICYCLE NETWORKS

*Develop local and regional bike plans, establishing a network of bicycle facilities to safely connect bicyclists of all abilities to schools, jobs, shops, transit stops, parks, and other destinations.*



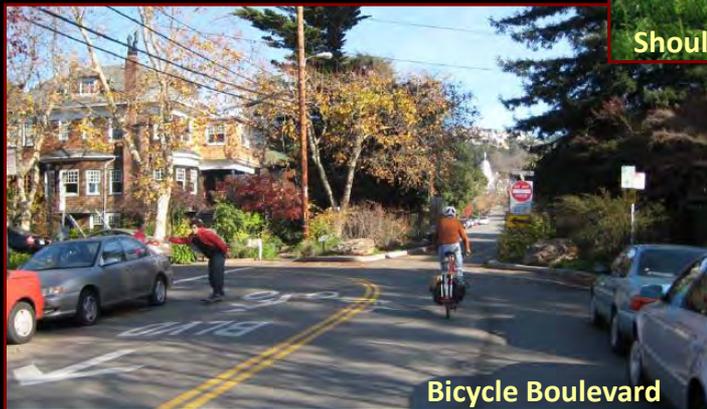
Shared Lane with Sharrow



Shoulder



Bicycle Lane



Bicycle Boulevard



Shared-Use Path

*Nationwide, half of all trips are three miles or shorter, less than a 20-minute bike ride, and one out of four trips is less than one mile, only a five-minute bike ride.*