

Historic Crossroads Redevelopment

The Crossroads area is centered around the Market and Main Street intersections with Albany Post Road, within a short walk of the Vanderbilt Site. It contains a unique concentration of sites listed on the National Register of Historic Places, including the Reformed Dutch Church, Hyde Park Firehouse, St. James Chapel, and the Main Street Historic District. This is also the civic core of Hyde Park, hosting the Town Hall and National Register Post Office and Library buildings. The 2005 Comprehensive Plan recommends re-establishment of the Crossroads as a center for historic and civic identity around a public green, emphasizing pedestrian movement while promoting economic development and tourism.

More than 10 years ago, the Crossroads Committee endorsed a rebuilding plan for the former Hyde Park Motors site at the center of the district. Hyde Park Walks has revived and updated that plan, adding more shared parking to the rear. The key concept is a triangular corner green that visually links the FDR-era stone Post Office and Library buildings. The new central green creates a historic heart for Hyde Park, with outdoor dining set back from Route 9 and a central space for a Roosevelt statue or other marker celebrating Hyde Park's history. Together, the redesigned commercial Town Center to the south and the Crossroads historic and cultural center can offer residents and visitors two prime destinations built around walkability and sidewalk-oriented businesses.



- ### Key Improvements
- Create corner park to visually connect FDR-era Post Office & Library
 - Fill in gaps with mixed-use buildings facing central green
 - Place shared parking lots to the rear of new buildings
 - Repair and extend sidewalk system using bold or textured crosswalks
 - Add on-street parking and street trees next to the curb to slow traffic and protect pedestrians



Hyde Park Walks

Hyde Park Town Center Pedestrian Study

A Report of the Hyde Park Walks Pedestrian Task Force

Assisted by the Dutchess County Department of Planning & Development

April 1, 2013

The Hyde Park Town Center includes several well-populated residential areas as well as the Town's main shopping district. Although some sidewalks exist, there is much that can be done to make the area more pedestrian friendly. The Town is actively pursuing redevelopment of the Town Center area into the vibrant, walkable community outlined in its comprehensive planning documents.

Planning Process – The Hyde Park Town Center Pedestrian Study was a community planning project conducted and funded through the Poughkeepsie-Dutchess County Transportation Council (PDCTC), with staff assistance from the Dutchess County Department of Planning and Development. Work included a GPS inventory of existing sidewalk conditions and materials, a public opinion survey, recommendations for future sidewalk/crosswalk improvements, and design alternatives for the historic crossroads and the signature area around Pine Woods Road and Route 9.

Sidewalk Inventory – Inventory results revealed that 95% of sidewalks in the area were in excellent/good condition. However, there were many examples of missing sections of sidewalk, lack of crosswalks at key intersections, and street ramps were often in deficient or damaged condition. These issues combine to make the area less pedestrian friendly.



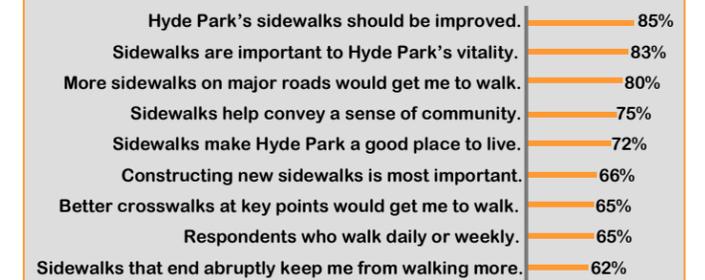
Although most of the sidewalks inventoried were in good or excellent condition, some areas need attention, such as the cracked, settled section on the left, and the disappearing section on the right.

Public Opinion Survey – A public opinion questionnaire developed and distributed by the Task Force elicited almost 300 responses. Over 65% of respondents walk daily/weekly, over 83% walk for exercise, and over 85% indicated they support making improvements to Hyde Park's sidewalks.



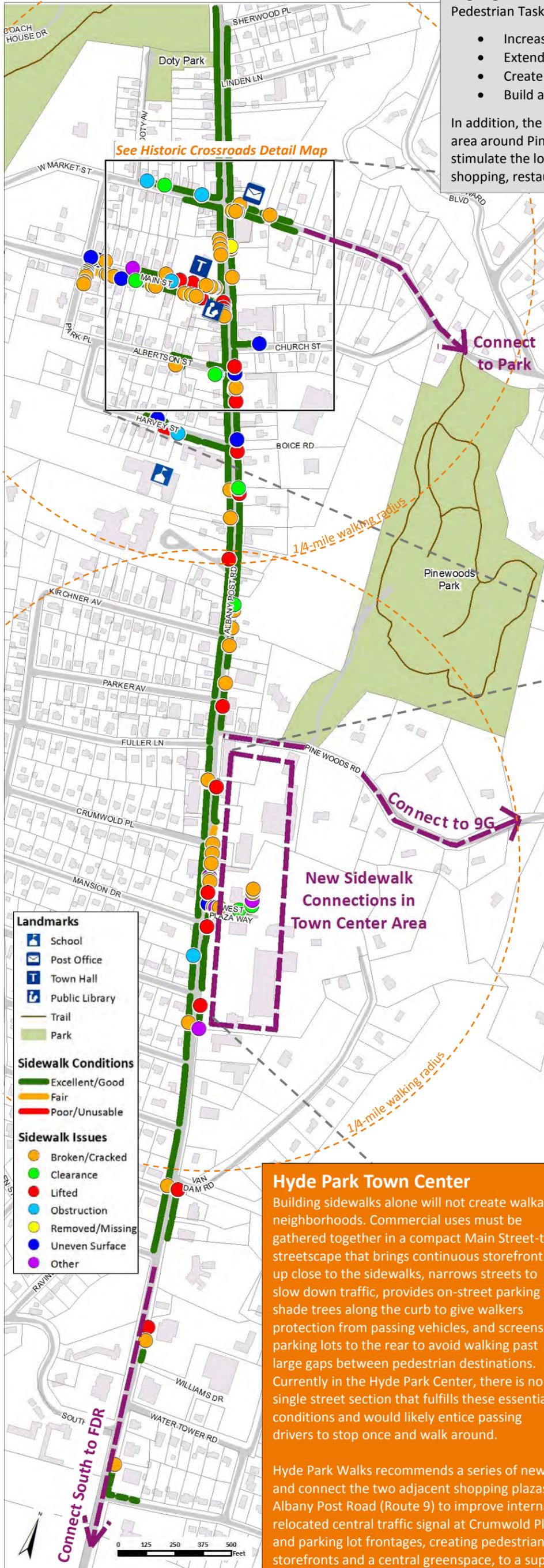
Wide roadways, numerous street and driveway intersections, and a limited number of crosswalks make crossing Route 9 in the Town Center a pedestrian-unfriendly experience.

Survey Response Highlights:



Some sidewalks end abruptly midblock [left]. Additional crosswalks would increase safety for pedestrians trying to cross Route 9 [right].

Sidewalk Inventory, Conditions, and Issues Map



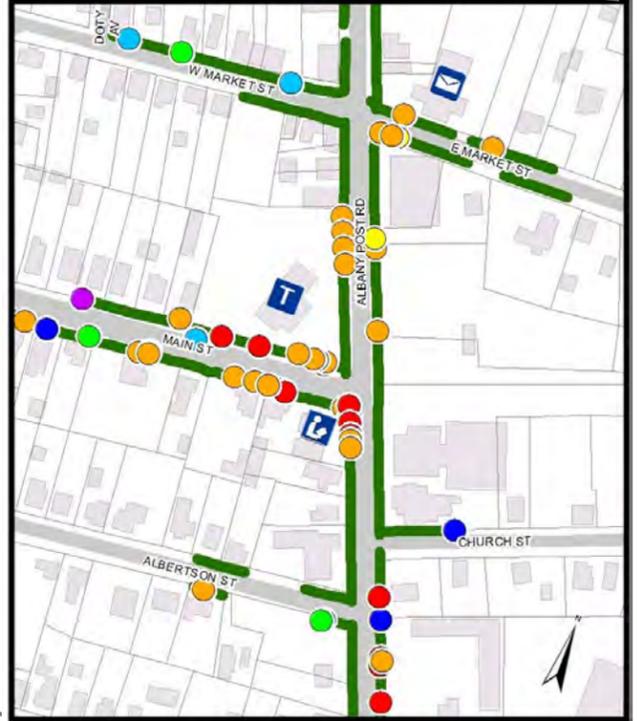
Sidewalk Improvement Strategy

Based on the results of the sidewalk inventory, public input, and in connection with the Town's ongoing efforts to improve pedestrian connections in the Town Center, the Hyde Park Walks Pedestrian Task Force identified four priority areas for improvement:

- Increase the number of sidewalk connections in the Town Center area;
- Extend sidewalks along East Market Street, possibly to Pinewoods Park;
- Create Pine Woods Road-to-Route 9G sidewalk connection;
- Build a sidewalk connection south to the FDR Home and Library.

In addition, the Task Force presents design alternatives for the historic crossroads and the signature area around Pine Woods Road and Route 9. By focusing on these improvements, the Town hopes to stimulate the local economy and enhance the quality of life by connecting residents and visitors to shopping, restaurants, and services in the area.

Historic Crossroads Detail Map



Key Improvements

- Tree-lined Route 9 with on-street parking
- Complete and connected sidewalk system
- Supermarket as new neighborhood anchor
- Street connection to light at Crumwold Place
- New sidewalk-oriented, mixed-use buildings
- Central green and tower at key intersection

Hyde Park Town Center

Building sidewalks alone will not create walkable neighborhoods. Commercial uses must be gathered together in a compact Main Street-type streetscape that brings continuous storefronts up close to the sidewalks, narrows streets to slow down traffic, provides on-street parking and shade trees along the curb to give walkers protection from passing vehicles, and screens parking lots to the rear to avoid walking past large gaps between pedestrian destinations. Currently in the Hyde Park Center, there is no single street section that fulfills these essential conditions and would likely entice passing drivers to stop once and walk around.

Hyde Park Walks recommends a series of new infill buildings with an integrated street, storefront, and sidewalk system to retrofit and connect the two adjacent shopping plazas south of Pine Woods Road. Two new Town streets are shown, one parallel to Albany Post Road (Route 9) to improve internal site circulation and divert some traffic off Route 9, and one east-west street with a relocated central traffic signal at Crumwold Place to ease left turns for everyone. New commercial buildings fill in the existing gaps and parking lot frontages, creating pedestrian-oriented streets linking the residential neighborhoods to the west, past continuous storefronts and a central greenspace, to a supermarket as a neighborhood commercial anchor to the rear. Infill buildings could be constructed incrementally, but consistent with the overall plan, thereby advancing economic development and creating several complete blocks as a mixed-use, pedestrian-friendly Town Center.